

## 2 Developing the Goods Yard

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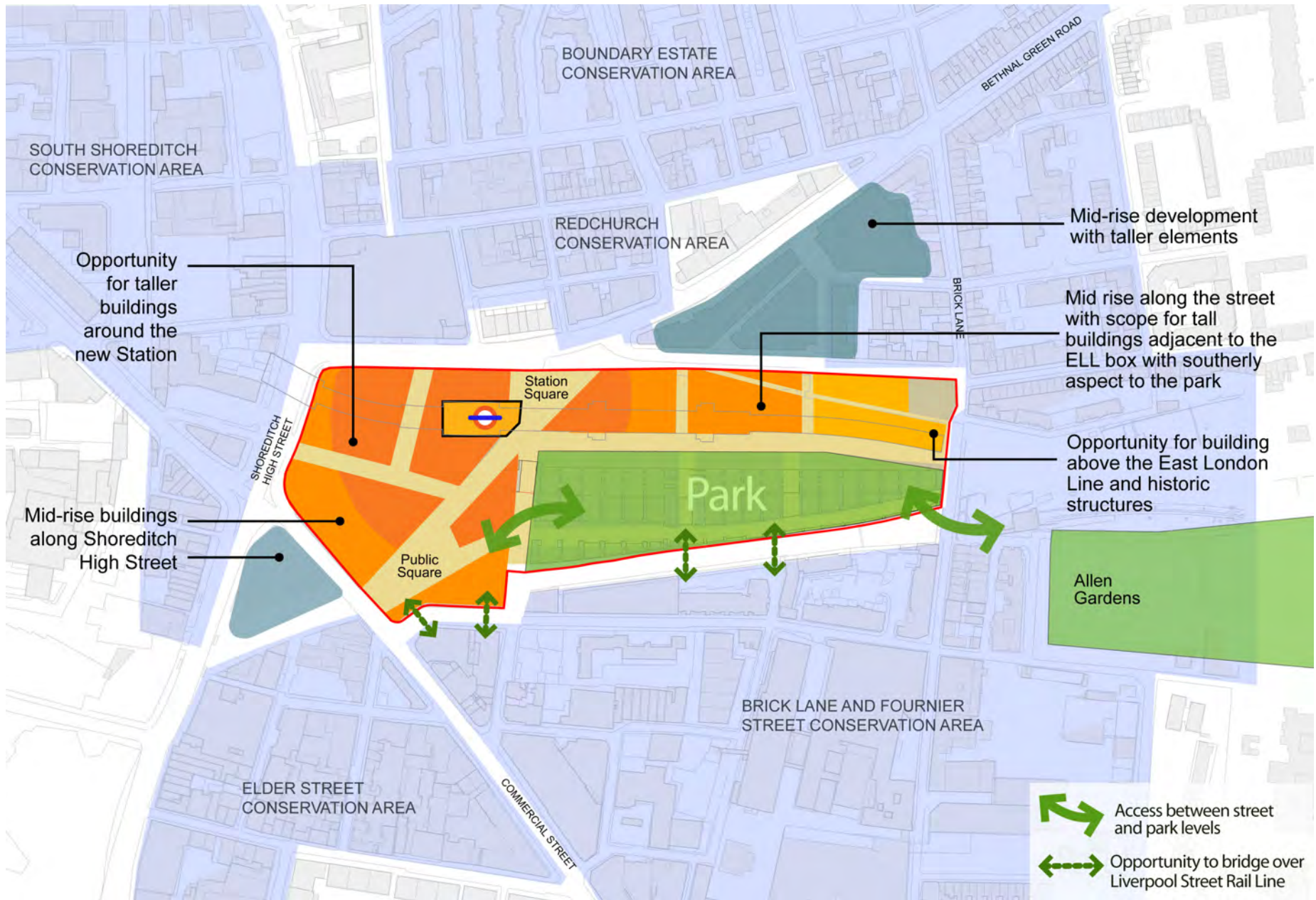


Fig. 20: Indicative vision for Bishopsgate Goods Yard

### The vision

2.1 Bishopsgate Goods Yard offers an exciting opportunity for a sustainable form of high density development closely linked to excellent public transport access.

2.2 The size of the site and relatively central location provides scope for a mix of uses and a balanced approach to development, which can:

- Contribute to supporting London's of financial and business services;
- Strengthen the local economy in Shoreditch and Spitalfields;
- Significantly contribute to local housing need;
- Provide an exciting place to live, work or visit;
- Be a place to be enjoyed by existing and new communities.

2.3 To achieve a balanced approach, to overcome physical constraints and ensure that future development is designed in an appropriate manner, it is important to establish overall development principles for the site.

2.4 The key features of potential development outlined in the planning guidance include:

- A development which encourages a more sustainable way of life and strives towards carbon neutrality and is adaptable to the effects of climate change;
- Development that provides a range of different housing types and sizes tenures, including larger family homes and affordable housing;
- A mix of employment opportunities including large offices and flexible spaces for small to medium enterprises (SME's);
- New community and healthcare facilities;
- Development that re-uses historic structures;

- Development that maximizes sustainable transport opportunities;
- Improves links through the site and provides access for all;
- Development that integrates with the surrounding area.

### Development principles

2.5 This section sets out the main design principles for the development of the Goods Yard, based on analysis of the site and surrounding area, opportunities, challenges and current planning policies. It will provide the essential guidelines that will shape development proposals coming forward and will be used in considering future planning applications for the site.

2.6 The design principles show how future development of the site could:

- provide new routes for pedestrians and cyclists through the site;
- connect new development into the surrounding area;
- provide a series of new public open spaces;
- take advantage of and maximise sustainable transport opportunities;
- bring historic structures back into use;
- respond to local character; and
- become an exemplary form of high quality sustainable development.

2.7 Detailed guidance is provided for the appropriate height of new buildings and how tall buildings could be located and designed.

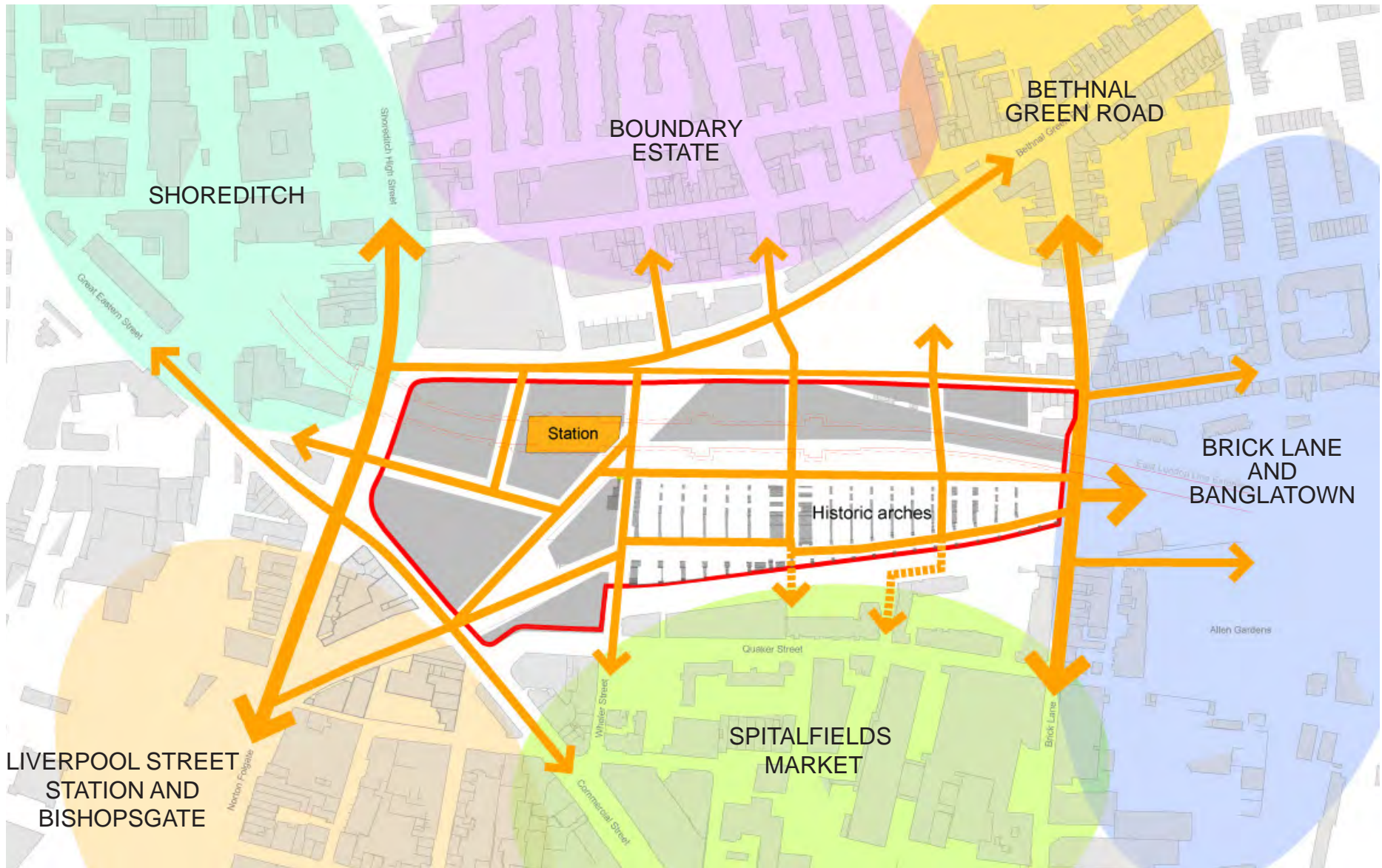


Fig. 21: The Goods Yard should integrate, physically, socially and economically, with the diverse communities surrounding it

### Connecting the wider area

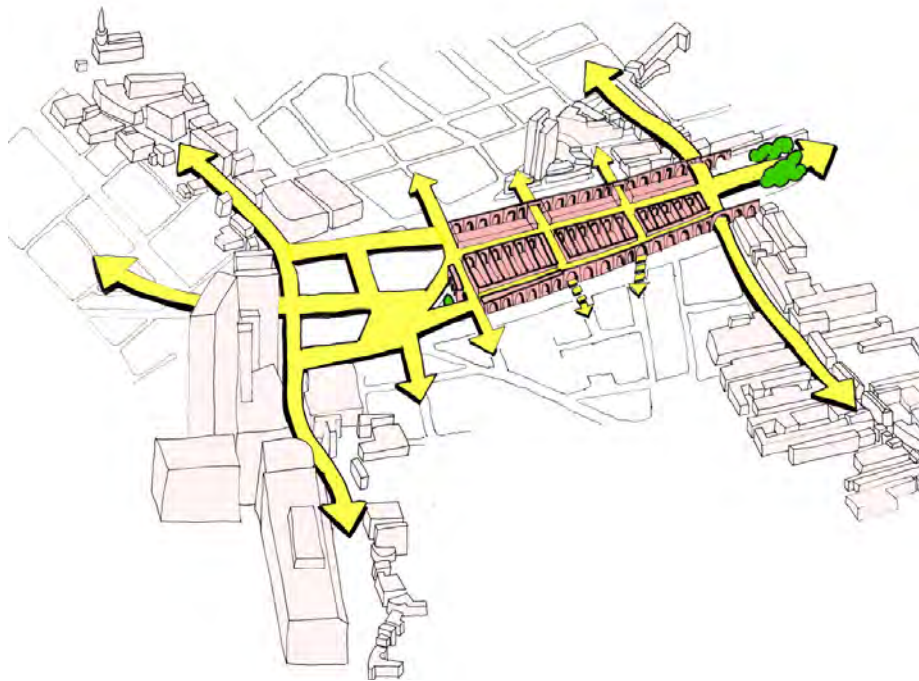


Fig. 22: New routes through the site would reconnect the wider area for pedestrians and cyclists

2.8 There are only two main routes that exist across the Goods Yard – Wheler Street, running north to south and London Road, the internal street that runs east to west beneath the Braithwaite viaduct. Both of these routes have been closed since construction of the East London Line extension commenced.

2.9 The size of the site and lack of public routes means that the goods yard is currently a barrier to movement around the area and is not well connected with the surrounding area. This contrasts with the much greater level of connection and opportunities for different routes and connections in the wider area.

2.10 Development of the Goods Yard should be based on the principle of creating a network of safe, well-designed and well-maintained pedestrian and cycle routes that would connect the site with its surroundings. New routes across the site should seek to follow the principles shown in figure 21 and as outlined in the guidance.

2.11 New development should focus main connections through the site:

- North to south by re-opening and upgrading Wheler Street for pedestrians and cyclists. Access for small vehicles to service the space beneath the Braithwaite viaduct could be provided at the southern end. General through access for vehicles should not be provided along Wheler Street.

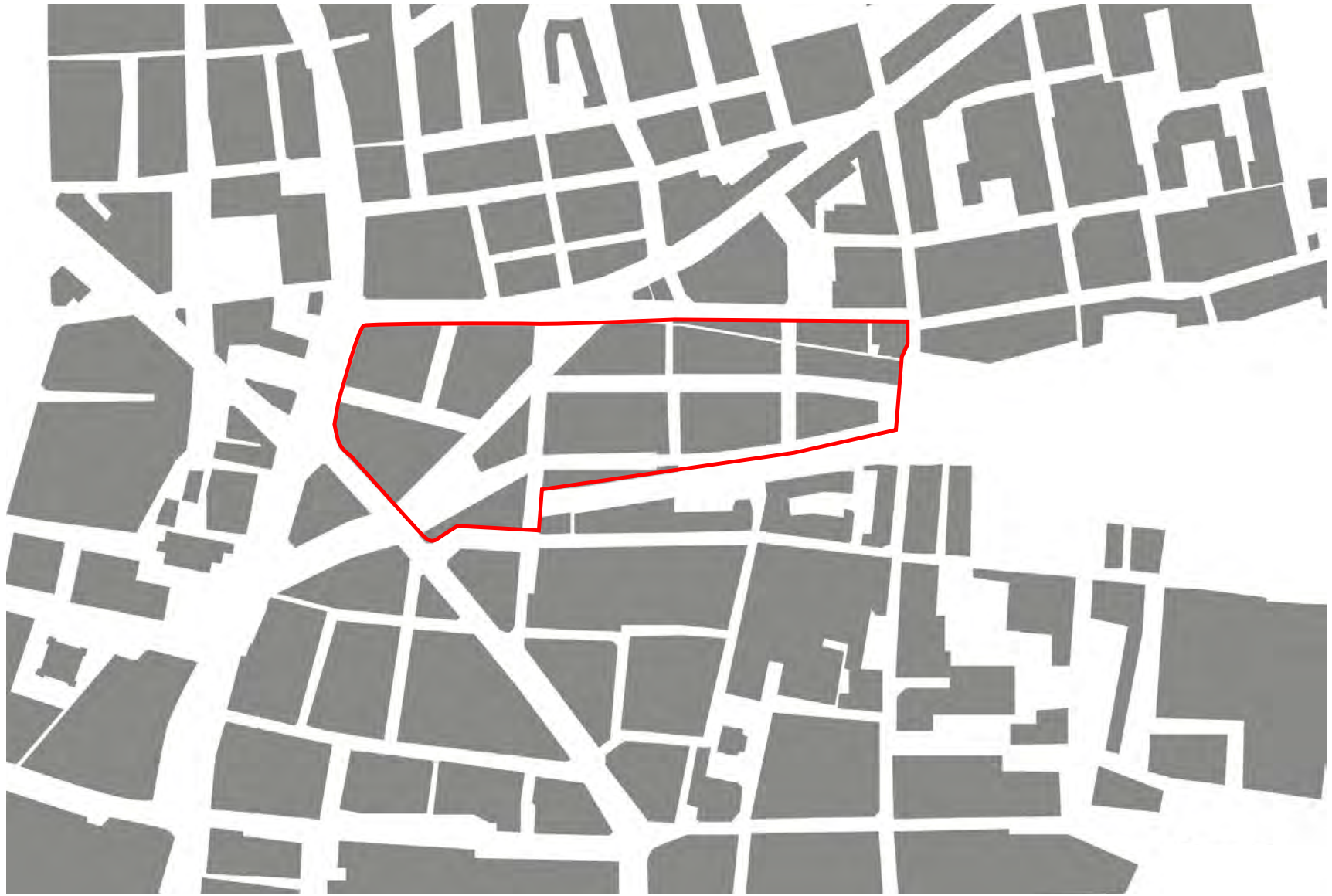


Fig. 23: An impression of the possible future urban grain of the area, with a new network of pedestrian routes across the Goods Yard

- East to west by re-opening London Road between Brick Lane and Wheler Street as an enclosed public street serving new shops, businesses, leisure and cultural uses in the arches beneath the Braithwaite viaduct. This route will need to be well lit and have good natural surveillance from uses located in the arches to feel safe and attractive.
- Creating secondary north south routes into the site from Bethnal Green Road and Sclater Street. These routes should, where possible, align with the planned public open spaces that would link Bethnal Green Road and Sclater Street through the re-development of the site at 32-42 Bethnal Green Road,

2.12 There are other opportunities to create new routes through the site as part of re-development proposals on the goods yard and on adjoining sites.

- Extending the main east west route from Wheler Street to Shoreditch High Street. This connection could incorporate the listed forecourt wall and gates as an impressive entrance feature to the site.
- Connecting the diagonal route between Bethnal Green Road and Norton Folgate could be achieved by bridging over the railway lines into Liverpool Street adjacent to the goods yard and also within the triangular site between Norton Folgate and Commercial Street.
- Creating secondary east-west routes between Brick Lane and Wheler Street running parallel to London Road and also cross the top of the Braithwaite viaduct through a new linear open space;

2.13 Opportunities should be explored to connect new routes from the north into London Road and up to the space on top of the Braithwaite viaduct, subject to appropriate design and obtaining listed building consent.

2.14 New routes should be aligned to ensure strong links can be provided to key destinations to reinforce existing movement patterns. For example a new north east to south west route could provide a more direct route through the site from the northern residential areas towards Liverpool Street Station.

2.15 By reconnecting the site with the wider area, Bishopsgate Goods Yard would play a key role in integrating, socially and economically as well as physically, the diverse neighbourhoods surrounding the site while ensuring that each area retains its own distinct character (figs. 21-22-23).



Fig. 24: Scale comparisons between areas of public space

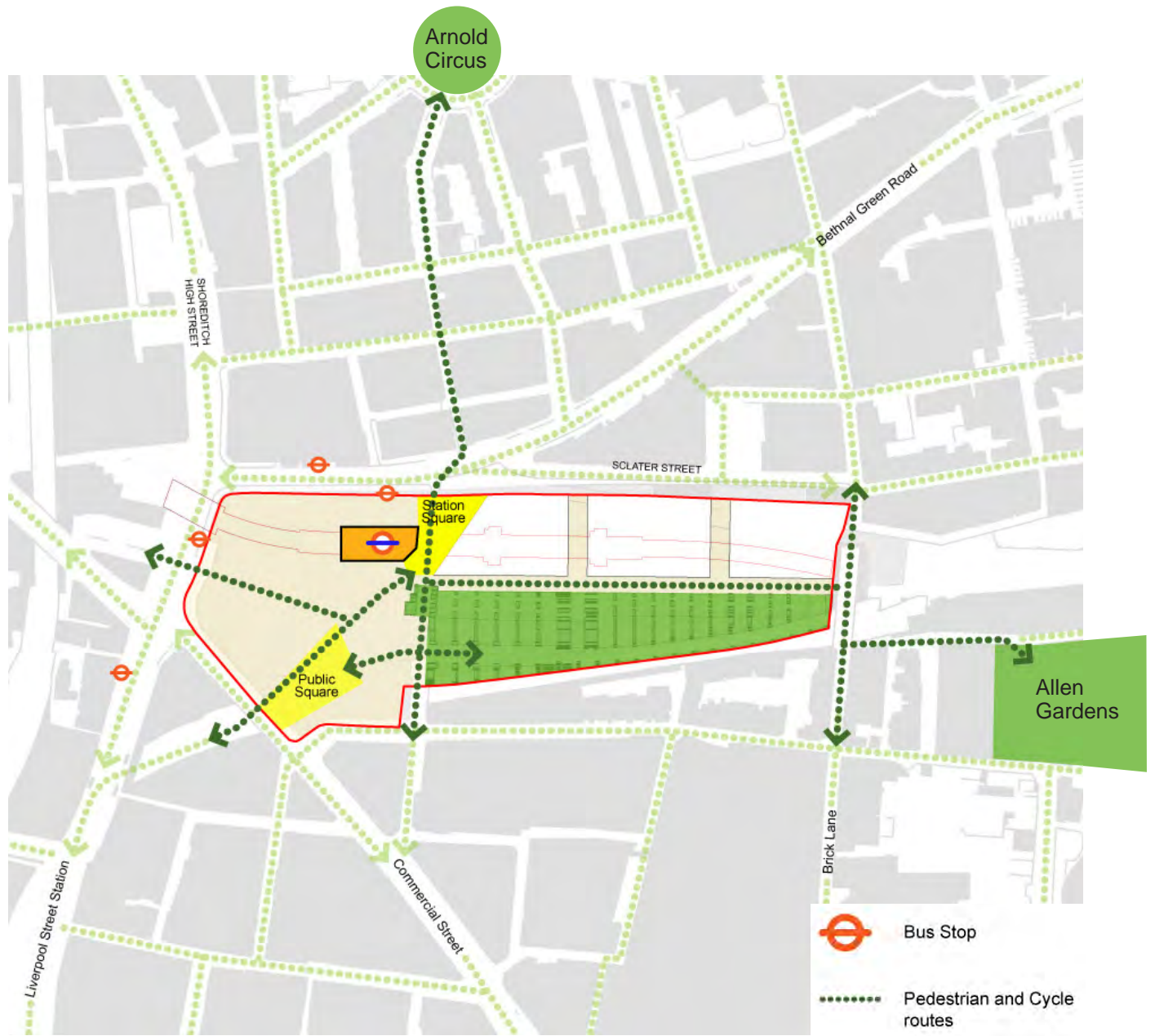


Fig. 25: Potential public spaces at Bishopsgate Goods Yard, linking with public space in the wider area



### New public spaces

2.16 There is a significant deficiency of public open space, green space and play-space in the wider area. The space above the arches is identified by LBTH as a Site of Borough Importance for Nature Conservation (SINC) but is not currently accessible to the public.

2.17 A series of new linked public open spaces for the local and wider community will provide opportunities for a range of activities from sport, recreation and play to rest and relaxation to be enjoyed by residents and workers.

2.18 Regeneration of the Goods Yard should address this deficiency by creating a new network of linked public spaces as shown on figure 25.

2.19 The main opportunities for new public open spaces are:

- A green park on top of the listed Braithwaite viaduct, with access for all as an integral part of the design;
- A new public square, opening on to Quaker Street, Wheler Street and Commercial Street as part of a new diagonal connection across the site, subject to bridging over the rail lines;
- A public forecourt to Shoreditch High Street Station at the northern end of Wheler Street, to improve the visual connection between the station entrance and Bethnal Green Road;
- A small urban square on Brick Lane to incorporate a connection to the higher level park above the Braithwaite Viaduct and link to Allen Gardens.

2.20 The new green space should become a major feature of the new public realm developed in consultation with the community and incorporate the following principles:

- A shared space accessible by all and a focal point for the local community;
- Designed to be safe, overlooked, well lit and well-maintained;
- Accommodate a variety of activities including recreation and children's play;
- Create a sense of community ownership, with the potential to be a venue for local festivals and other events, encouraging integration of the development into the wider area; and
- Enhance local biodiversity by creating potential habitats for black redstarts and other species.

2.21 As part of the redevelopment of the Goods Yard, the other new public spaces should be designed to:

- Provide a setting for the buildings around them;
- Integrate active uses such as shops, restaurants or community facilities in the ground floors of adjacent buildings;
- Have distinct but different characteristics;
- Be accessible to all;
- Provide for a range of different activities to meet the needs of the local community.

2.22 Innovative and exciting forms of public art should be provided to enliven public spaces. Local artists should be encouraged to contribute to public art projects.

2.23 A public realm strategy should be devised to guide the design and future management of all the public open spaces that could be developed within the goods yard. This could come forward to support the planning application(s) for the first phase of development or as a separate document.

## Sustainable transport

2.24 The new Shoreditch High Street station will open in 2010 providing improved access to the East London Line and to the wider over ground and underground rail networks. Bishopsgate Goods Yard is also well served by frequent bus routes along Bethnal Green Road, Commercial Street and Shoreditch High Street.

2.25 The integration of public transport and pedestrian movement will be vital to the successful regeneration of the site. New safe pedestrian and cycle routes should be created, reinstating lost connections, linking with and improving the existing provision in the wider area.

2.26 The goods yard site could become a new public transport point for the wider area, improving permeability and access for all, and encouraging the use of sustainable transport methods. The movement strategy for Bishopsgate Goods Yard will be based on encouraging people who come to the area to do so on foot, by cycle or by public transport, promoting sustainable transport options and reducing the need for the private car.

2.27 Development should be designed so that:

- Shoreditch High Street station remains highly visible and accessible from Shoreditch High Street and Bethnal Green Road at all times;
- New links to the station are provided through the site from Brick Lane;
- A new high quality open space is provided outside the station entrance;
- Changing between buses and East London Line trains is more straightforward, including the relocation of existing bus stops around the site;

- There is provision of space and support for a car club linked to accredited schemes operating in the area;
- New and existing routes to the station are safe, well lit and benefit from natural surveillance during the day and night;
- A range of cycle storage, workplace shower and locker facilities and maintenance facilities are provided across the site;
- Priority is given to walking and cycling on new routes through the site;
- Wheler Street remains closed to through traffic, except for emergency vehicles or controlled servicing arrangements;
- The development seeks to be car free except for necessary provision for people with disabilities, and controlled servicing requirements;
- Drop off points for people with disabilities and taxis are provided close to the station and main building entrances;
- TfL London Cycle Hire scheme docking stations and off-street electric vehicle charging points are provided within the development or close to the site; and
- Delivery and servicing arrangements can be successfully accommodated.

The opportunity to improve bus standing and driver facilities in the vicinity should be discussed in more detail with Transport for London and London Buses.

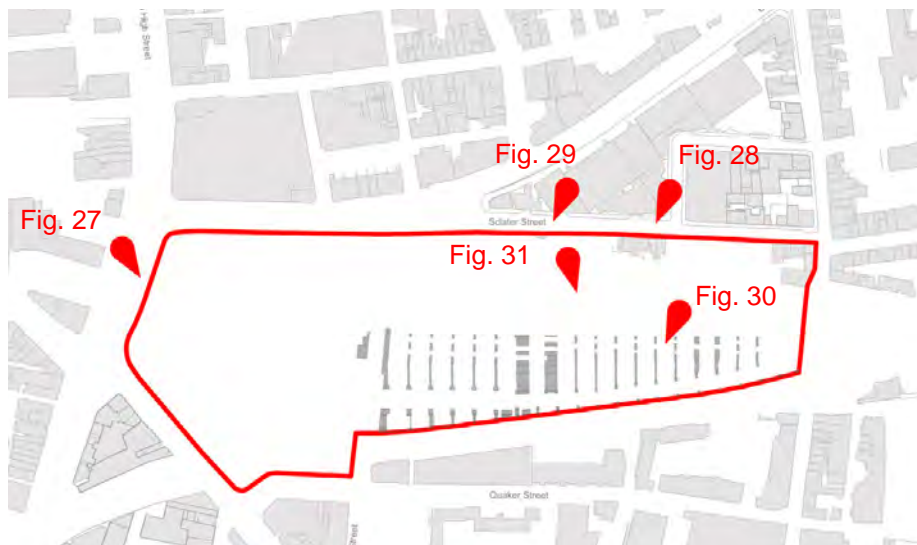


Fig. 26: Computer-generated image of the new East London Line station

### Re-using the historic structures



Fig. 27: Grade II listed oriel on Shoreditch High Street



2.28 Bishopsgate Goods Yard has a particularly interesting history, and the structures remaining on the site help define the site's unique character (fig's 27 to 31). These include the listed Braithwaite viaduct, the listed forecourt wall, and gates to Shoreditch High Street, the railway arches west of Wheler Street, the former weaver's cottages on Sclater Street and the boundary wall to Sclater Street.

2.29 Existing buildings and structures should be seen as valuable resources, and many examples exist where regeneration has successfully integrated new development with historic buildings and structures from our industrial heritage which add character and texture. Historic continuity and recognition of a site's past history are important components of creating a successful sense of place. Proposals should consider opportunities for providing education and information on the history and heritage of the site.

2.30 Government guidance in PPG15 Planning and the Historic Environment, notes that most historic buildings can be put to good economic use. The retention of historic buildings and structures adds to the quality and enjoyment of the built environment. Regeneration proposals should consider the opportunities to reuse these structures in their existing locations and to enhance their setting in order to help shape, define and support a new and vibrant public realm and provide continuity between the past and the future.



Fig. 28: Unlisted weavers' cottages on Sclater Street



Fig. 29: Unlisted historic wall along northern site boundary

2.31 Development on the site should enhance and integrate the listed structures by:

- refurbishing and re-using the arches beneath the Braithwaite viaduct for shops, leisure and culture uses and to provide potential space for small businesses and creative industries.
- the area on top of the Braithwaite viaduct provides an ideal opportunity for a successful green open space;
- reusing the historic arches would create active frontage along the new streets across Bishopsgate Goods Yard, ensuring that this environment will be well-used, vibrant and safe;
- considering how the listed forecourt wall and gates could be retained and integrated into the development, for example to mark the main route into the site from Shoreditch High Street.

2.32 Other historic structures should be retained and incorporated where possible. The former weaver's cottages on Sclater Street are an important feature of the northern part of the Brick Lane and Fournier Street Conservation Area. These buildings should be retained if possible and brought back into use contributing to the mix of uses including new homes that could come forward on the site.

2.33 The brick boundary wall on Sclater Street includes attractive brick arches. The wall should be incorporated within the development, for example by helping to define the edge of the station square.

2.34 Demolition of the unlisted brick arches on the western part of the site may be appropriate where it would help to increase permeability and provide connections to the adjacent streets.

2.35 The arches beneath the Braithwaite viaduct and potentially those on the western part of the site also provide an opportunity to accommodate appropriate temporary uses after Shoreditch High Street Station opens but before the main development takes place, ensuring that regeneration of the site and improvements to connections can begin as early as possible.

2.36 Retention of the historic structures will ensure that local distinctiveness and the historic significance of the site is maintained and will help to mediate between the different scales and typology of new and existing structures.



Fig. 30: Grade II listed : Arches beneath Braithwaite Viaduct



Fig. 31: Grade II listed Braithwaite Viaduct



Fig. 32: A unique and exciting place created by reusing historic structures (Image shows indicative building plots)



Fig. 33a Redchurch Street

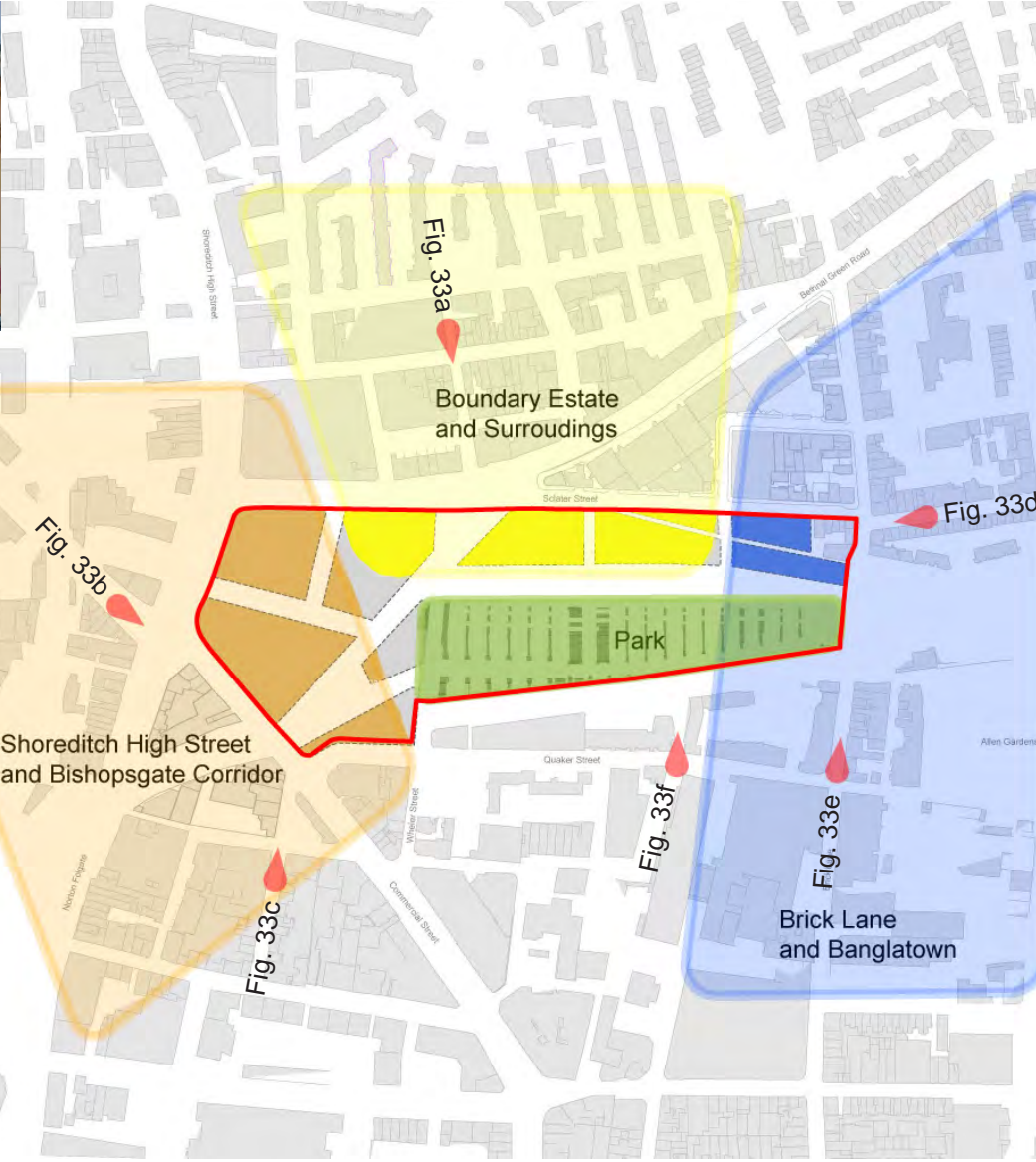


Fig. 33d Cheshire Street



Fig. 33b Great Eastern Street



Fig. 33e Brick Lane



Fig. 33c Elder Street

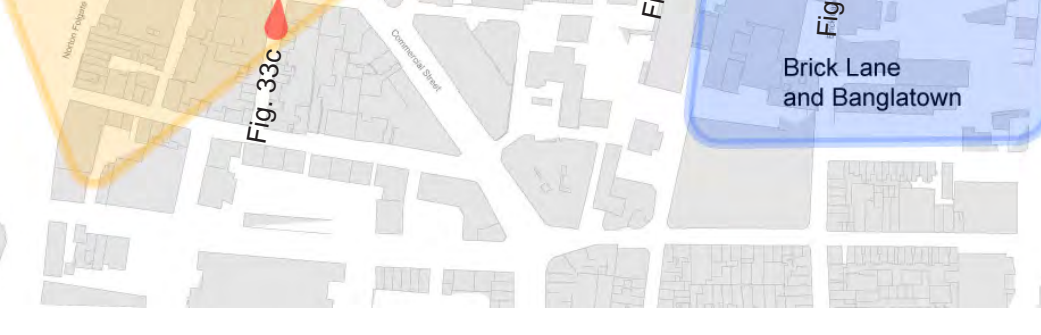


Fig. 33f Cheshire Street

Fig. 33: Bishopsgate Goods Yard - existing views from surrounding streets



### Responding to local character

2.37 Bishopsgate Goods Yard already has the advantage of being located close to areas of strong and distinct local character, accommodating many diverse communities, different economic activities, uses and architectural forms (as shown in figure 32).

2.38 Regeneration of Bishopsgate Goods Yard has the potential to strengthen the local character of the area, by providing new facilities, spaces, uses and buildings that respond to the existing context.

2.39 Redevelopment could strengthen local character by providing a mix of uses including:

- Uses that provide activity during the day and into the evening;
- Space for small scale shops to meet day to day needs and more specialist retail
- Flexible space for small and medium sized businesses, to encourage the continued growth of the cluster of creative industries in Shoreditch and Spitalfields.

2.40 Restaurants, café and bars will be appropriate within the goods yard, where they help to ensure appropriate levels of activity into the evening and subject to there being no increase in noise or disturbance to local residents.

2.41 Proposals should reinforce the positive aspects of the area, responding to the existing urban environment and street pattern by taking into account:

- Building footprints;
- Rhythm of architecture and building facades;
- An appropriate width of spaces between buildings; and
- A variety of architectural approaches to create a genuine, interesting and authentic place.

2.42 Bishopsgate Goods Yard has a particularly strong historical context. Proposals should respond to this through:

- Re-use of historic structures on the site;
- Improvements to the setting and access to historic structures;
- Provision of opportunities for education on the historic environment of the goods yard and surrounding area.

2.43 Redevelopment should respond to the ability of the area to successfully integrate old and new, as well as the ability of the area to integrate architecture of different scales and diverse form, linking with the existing context of the City Fringe to the west of the site, as another aspect of local character.

2.44 Proposals should improve the streetscape of the surrounding area, and the provision of public realm both on and around the site. Regeneration of the Goods Yard should contribute to the improvement of public open space, conservation areas and the built environment in Shoreditch and Spitalfields.



Fig. 34: Diversity of buildings from the surrounding area

## Building heights

2.45 The site spans from Shoreditch High Street to the west through to Brick Lane in the east. The varied context of the immediate areas presents opportunities for a variety of building scales and architectural character. The planning guidance presents a strategy for the height of buildings across the goods yard based on an assessment of the planning policy context, regeneration potential and having regard to strategic and local views.

2.46 The key guiding principle is that there will be transition in scale and building height across the site from west to east. The tallest buildings should be sited west of Wheler Street, with transition in scale to Brick Lane.

2.47 The location and design of any tall buildings on the western part of the Goods Yard will need to be carefully managed and evaluated against the criterion set out in this section of this document. The guidance and criteria in this section follows closely the Guidance on Tall Buildings, published by English Heritage and CABE in 2007.

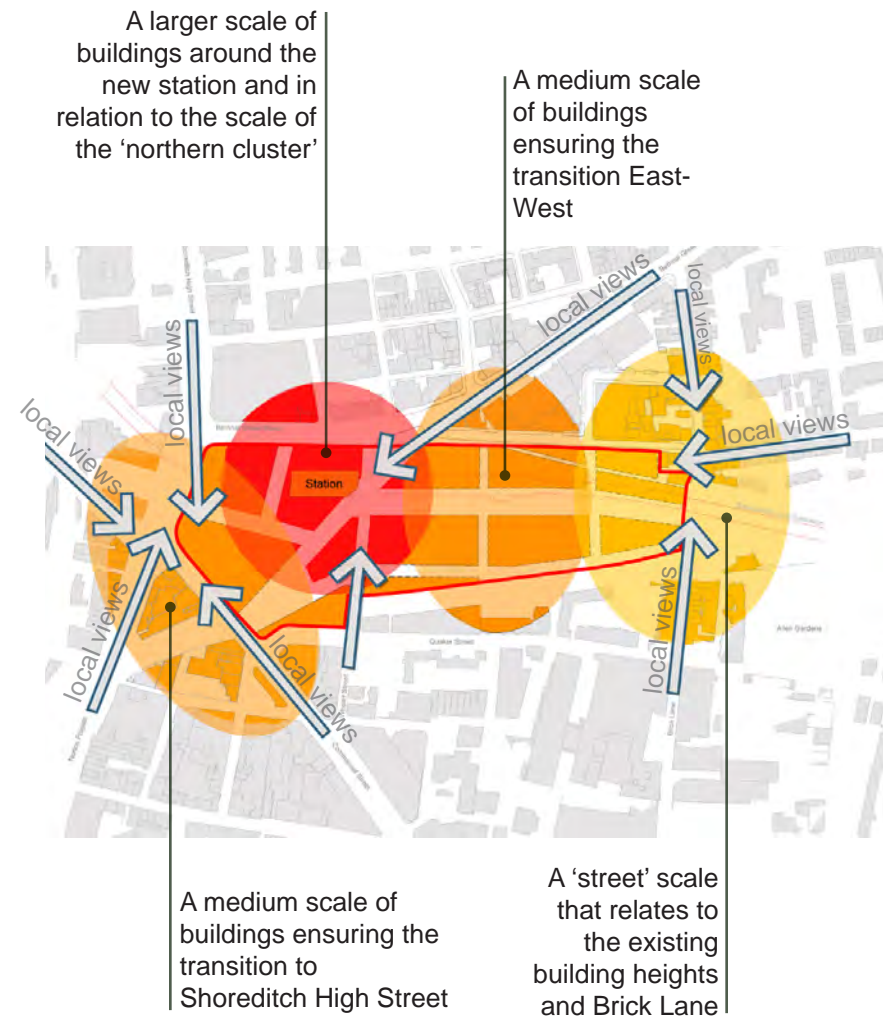


Fig. 35: Diagram showing indicative transition in scale of buildings from west to east across the site

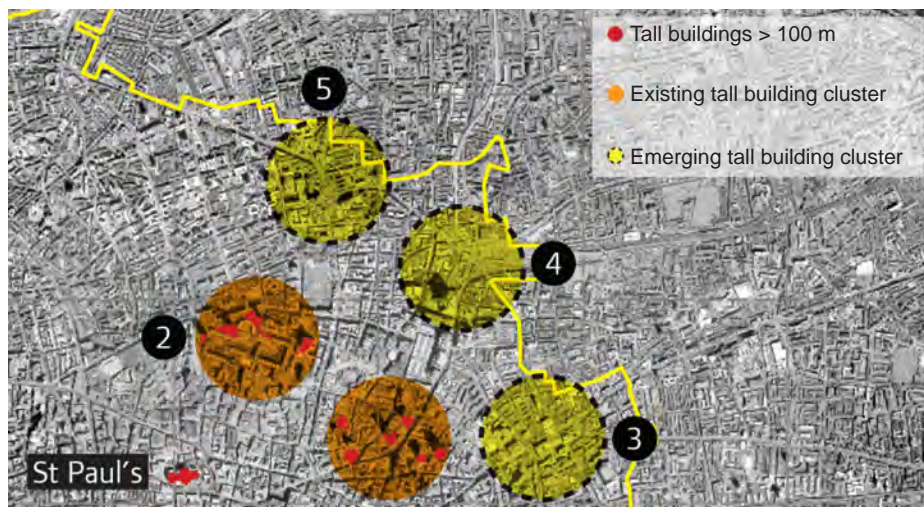


Fig. 36: Map showing areas (indicated as shaded) designated as suitable for tall buildings (from City fringe OAPF). Bishopsgate indicated as part of location 4.

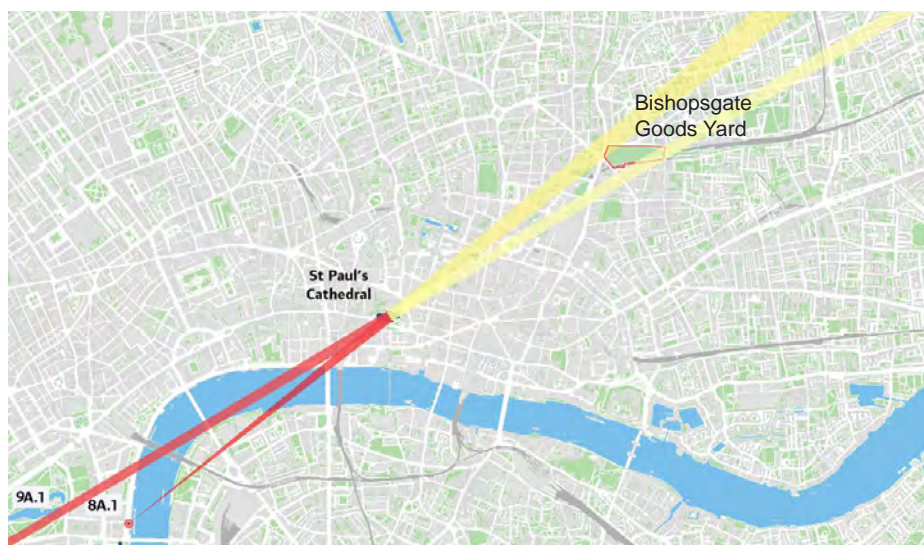


Fig. 37: Strategic view corridors affecting development of the Goods Yard

## Where would tall buildings be appropriate?

### Planning policy context for tall buildings

2.48 The London Plan states that the Mayor will promote the development of tall buildings where they create attractive landmarks, enhancing London's character, act as a catalyst for regeneration and where they are also acceptable in terms of design and impact on their surroundings. Tall buildings can be an efficient way of using land and can contribute toward creating a sustainable world city.

2.49 The Mayor's draft City Fringe Opportunity Area Planning Framework identifies a number of locations within the City fringe as having potential for tall or larger scale buildings, including Bishopsgate Goods Yard. The City fringe has always been characterised by a close proximity of different uses, built form and scale. The draft OAPF also sets out the threshold heights above which tall buildings, would be visible in strategic views.

2.50 The South Shoreditch SPD, Hackney Tall Buildings Strategy and the emerging Hackney Core Strategy identify the western part of the site as a tall building opportunity area.

2.51 Tower Hamlets has planning guidance policies for assessing the location and impact of tall buildings. Tower Hamlets' City Fringe AAP (adopted planning guidance) shows "building heights reducing with distance from the Goods Yard. Buildings heights should have an effective transition, decreasing from the employment focus in the west to residential focus in the east."

### Relationship to transport infrastructure

2.52 Bishopsgate Goods Yard site includes the new Shoreditch High Street station on the East London Line. This major transport investment together with access to a network of bus routes makes the site very accessible by public transport. The western part of the site achieves the highest possible public transport accessibility level (PTAL), which the London Plan uses to guide appropriate development density. This makes the site appropriate for high density development, which when combined with the physical constraints of the site would support the case for taller buildings.

### Regeneration potential

2.53 The emerging Hackney Core Strategy identifies the City Fringe as a growth location where existing and programmed infrastructure can best support significant economic and housing growth.

2.54 Bishopsgate Goods Yard is the largest and strategically located development site within the City Fringe which has been vacant and disused for over 40 years.

2.55 To maximise the appropriate density for the site, in accordance with the London Plan and to generate a viable development, buildings that are significantly taller than their surroundings may be required.

2.56 Maximising density by including some tall buildings will help to increase the contribution that redevelopment can make towards improving community facilities, for example by providing a large area of green space or through contributions to community facilities in the surrounding areas.

### Local topography

2.57 The City Fringe including Bishopsgate Goods Yard site has a generally level topography with a slight rise from the River Thames edge towards the north. There is a significant rise occurring to the west and northwest of the City Fringe towards Angel and up to Holborn. As a result there are extensive linear views into the City Fringe along many of the key routes, for example Whitechapel Road, Mile End Road, Commercial Road and Kingsland Road. 30 St Mary Axe (the Gherkin) appears at the end of many of these views, marking the transition to the City of London.

2.58 Tall buildings located on Bishopsgate Goods Yard could have a similar positive impact to 30 St Mary Axe. Bishopsgate has historically acted as a gateway to London and the potential exists, generated by topography and the views along the main routes of Shoreditch High Street, Commercial Street and Great Eastern Street to mark the northern gateway from Shoreditch to the City with a key landmark building.

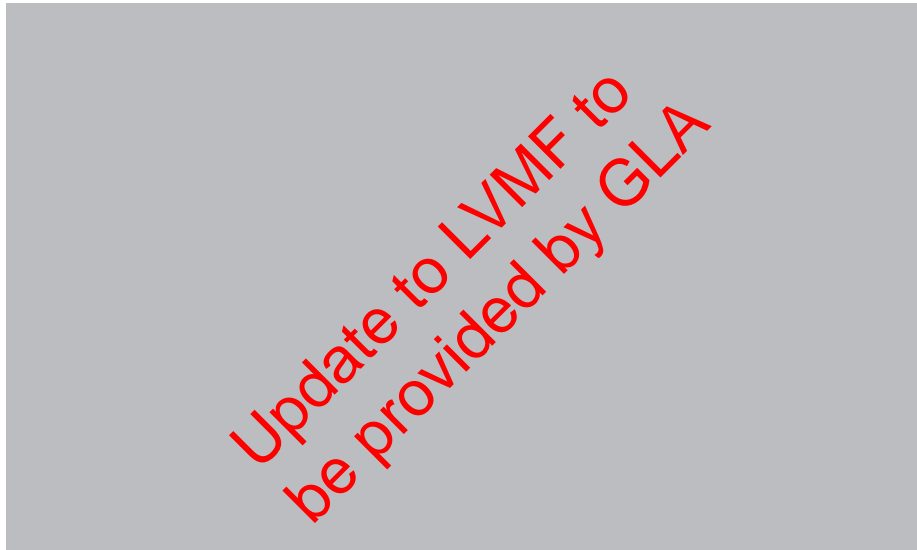


Fig. 38: Map showing approach to tall building locations by the London Borough of Hackney and Tower Hamlets and the GLA (from the city fringe OAPF)

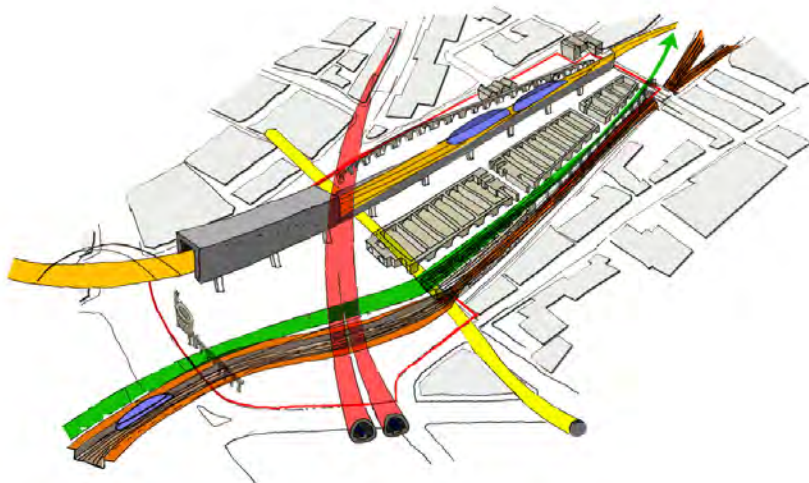


Fig. 39: Physical constraints viewed together

### Physical constraints

2.59 The concentration of foundable area towards the west of the site, and the policy designation of the site as an appropriate location for tall buildings, suggest that the western end of the site is most suitable for the location of tall buildings, with a diminishing transition in scale towards Brick Lane. Wheeler Street provides a natural defining edge and transition point, east of which the buildings descend in height and are of medium to low scale.

2.60 The presence of the East London Line, above ground, across the site means that there will be a structure of around six residential storeys height across the northern part of the site'. The East London Line has been boxed in to allow comprehensive development adjacent to and around this structure. (fig.39)

2.61 One of the factors impeding re-development of the site is the physical constraints imposed by main line and suburban rail tracks, Central line and BT telecommunications tunnel running below the site. This concentrates the area where substantial development could occur the west of site.

### Strategic views

2.62 The Draft Revised London View Management Framework Supplementary Planning Guidance (June 2009) includes two protected vistas to St. Paul's Cathedral which impacts the site and the wider City Fringe area thereby restricting height.

2.63 The area that falls between the two view corridors presents a restricted opportunity for a carefully managed approach to the location and design of tall buildings in this area. The western half of the goods yard lies within this zone and would have minimal impact on the strategic views.

### Local views

2.64 The location and design of any tall buildings on the goods yard will need to take into account the impact on local views, particularly those from within the adjacent conservation areas. Tall buildings have the potential to enhance the setting of conservation areas such as South Shoreditch. Tall buildings have the potential to create interesting and dramatic contrasts between high quality new architecture and historic townscape in views from Elder Street to the south and Redchurch Street Conservation area to the north.



Fig. 40: View from Shoreditch High Street, looking towards the City

2.65 Other important views include those along main approaches to the site, for example along Commercial Street, Shoreditch High Street and Bethnal Green Road. Figure 33 (p.44) shows the direction of key local views that must be taken into account in the location and design of tall buildings on Bishopsgate Goods Yard

#### *Relationship with the South Shoreditch Conservation area*

2.66 The strategy for building heights promotes a transition in scale. The tallest buildings would be most appropriately located between Wheler Street and Shoreditch High Street. The relationship between tall buildings and South Shoreditch conservation area will need to be carefully considered

2.67 The dramatic change in scale between the typical 4-6 storey Shoreditch High Street buildings and the taller buildings in the city, is a distinctive characteristic of South Shoreditch Conservation Area, and further emphasises the vertical character of the area.

2.68 The management approach towards South Shoreditch Conservation Area is to maintain a consistent scale and height of buildings within the Conservation Area boundary, and to support in principle taller developments beyond its boundaries within the City fringe to accommodate the development pressures.

2.69 This balanced approach provides a framework for achieving economic benefits while preserving historic environment. The urban design relationship between tall and larger scale buildings with the finer scale and grain of South

Shoreditch will require careful consideration and a detailed design response. Any proposals for tall buildings should comprise a carefully studied architectural response based on robust urban design principles set in this guidance to achieve a positive relationship between the two distinct character areas.



Fig. 41: Two scales of architecture co-existing looking towards the City from Bishopsgate



### Design guidelines for tall buildings

2.70 Bishopsgate Goods Yard site is a historically important site with listed structures and buildings and surrounded by areas with rich historic fabric including five conservation areas. This historic character of the site and its surrounding should inform the character of the new development.

#### Local views and skyline

2.71 Tall buildings would have a significant impact on local views and would be visible from and form background to parts of South Shoreditch, Elder Street, and Redchurch Street conservation areas. The height and volume of any tall buildings should be designed to present a carefully modelled massing when viewed from the adjacent conservation areas. Any proposals will need to include detailed analysis of the impact on local townscape and views analysis to enable proper consideration.

#### Townscape

2.72 The site adjoins Shoreditch High Street, Bethnal Green Road, Sclater Street and Brick Lane. The design and frontage of proposed development along these streets is central to integrating the development into its context. This is particularly crucial when considering tall buildings. Tall buildings should be setback from the main street edges, either on a podium or behind lower rise buildings.

2.73 The podium or lower rise buildings should be designed to relate to the building heights of the surrounding context, with a human scale to generate a successful streetscape in terms of the specific character of each of the streets including plot width, street rhythm, materials and elevation design.

2.74 Shoreditch High Street is a principle route within the district and a historically important street showcasing some of the finest warehouses and showrooms relating to the Victorian furniture industry. Redevelopment of the goods yard presents an opportunity to restore and reutilise the main forecourt wall, entrance gates and oriel. Tall buildings should be set back from the edge of the high street and meaningfully integrate the listed structures and adjacent arches to create a successful and exciting mix of historic and new development.

2.75 Bethnal Green Road is another important road off Shoreditch High Street and the main route to the new ELL station. Tall buildings should be set back from the street edge. Lower rise buildings that front Bethnal Green Road should where possible relate to the immediate context including the buildings across the street which range from four to eight storeys. The street frontage should accommodate uses that create and encourage a safe, lively route to the station, including generous footpath to accommodate potentially high levels of pedestrian movement. Careful consideration should be given to the design of the corner of Shoreditch High Street and Bethnal Green Road to draw people towards the station entrance.

2.76 Sclater Street is a connecting route between Bethnal Green Road and Brick Lane. The red brick former boundary wall containing ornamental arched openings is visible close to the junction of Sclater Street and Bethnal Green Road. This potential to retain and incorporate some or all of this historic feature should be considered as part of any future development.

2.77 To the northeast of the site is a group of former Weaver's cottages at 100 to 106 Sclater Street. These buildings are an important remnant of the former character of the area and should be retained, refurbished potentially for residential use and incorporated into the wider development.

2.78 The East London Line viaduct box is runs parallel to Sclater Street and is clearly visible from the immediate area and also from key views within Redchurch Street and Boundary Estate conservation areas. There will be scope for taller development set back from the Sclater Street frontage buildings where it would mask the railway box, create visual interest and enhance local views.

2.79 Brick Lane: The goods yard adjoins Brick Lane with a brick boundary wall that includes entrance gates to London Road (an internal route alongside the Braithwaite Viaduct, The height strategy does not anticipate any tall buildings in this location. Removal of part of the wall to create a new public space on Brick Lane providing access to the park above the Braithwaite viaduct and to open up east west route along London Road would be appropriate.

### Architectural and design quality

2.80 As discussed earlier the site context is predominantly composed of high quality historic fabric most of which falls within a conservation area. Locating tall buildings creates a challenge regarding the relationship of tall and large scale buildings to the finer scale and grain of the wider setting.

2.81 Architectural treatment should avoid large-scale horizontal or flat elements that might emphasise building width. Design should achieve a highly modelled, richly layered elevation treatment which generates a visually interesting architecture and is sympathetic if not consistent with its wider context. Due consideration should be given to the tops of the building(s) and how they contribute to the skyline. Design should include the use of high quality of materials and detailing.

### Sustainable design and construction

2.82 Tall buildings should set exemplary standards in design because of their high profile and local impact. Proposals should meet or exceed the latest building regulation and planning policies for minimising energy use and reducing carbon emissions over the lifetime of the development. The long-term resource and energy efficiency of tall buildings will be enhanced if their design can be adapted over time and it is able to accommodate latest developments in environmentally sustainable technology.



Fig. 42:  
Spaces and routes activated  
by public uses at ground level

### Effect on local environment

2.83 Tall buildings should be sited carefully to avoid heightened sense of enclosure and so as not to feel overbearing at ground level. The location of tall buildings must not create unacceptable impacts on the amenity of existing or future residents in terms of access to daylight and sunlight. This will be particularly important in terms of the residential character of the area to the north of the goods yard. Tall buildings should not create uncomfortable environmental conditions, for example high wind speeds or long periods of overshadowing for pedestrians at street level and in public open spaces, either adjacent to or within the development site.

### Contribution to public spaces and facilities

2.84 Tall building proposals should both internally and externally provide a mix of uses especially at ground floor level, and dedicate these areas as far as possible to public use. Opportunities should be provided for members of the public to enjoy views afforded from the upper floors of tall buildings. Proposals for tall buildings on the western part of the goods yard must include the delivery of public open space adjacent to them (for example the urban square or the higher level park), to meet the needs of future occupiers, to provide a setting for the buildings and to mitigate the impact of high density development. Planning agreements will be used as a mechanism to secure the proposed public open spaces.

### Contributing to permeability

2.85 Tall building proposals should offer improved permeability, accessibility and legibility of the site and the wider context. Bishopsgate Goods Yard site, due to the nature of its historic function has been disconnected from its surroundings. Redevelopment of the site is an opportunity to introduce routes and linkages to adjoining streets to re-connect the site with its surroundings. Tall building proposals on the western part of the site should allow for effective linkages to Shoreditch High street and Bethnal Green Road. The character, design and hierarchy of these routes should reflect and be informed by the hierarchy and importance of the existing network of routes. For instance Shoreditch High Street is a major arterial route within this part of Hackney and the routes from the high street should therefore appropriately reflect its importance.

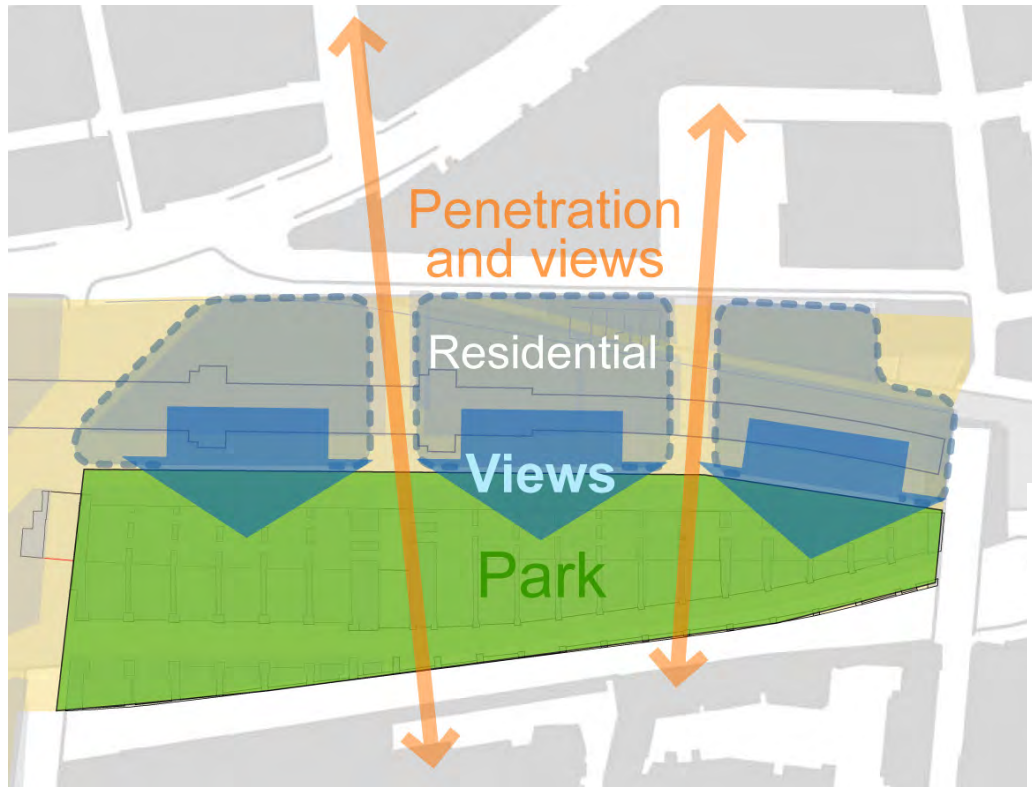


Fig. 43: Penetration of daylight and sunlight should be encouraged by ensuring there is not a 'wall' of development along the northern site edge.



Fig. 44: Aerial photograph showing Bishopsgate Goods Yard within its urban context, with Conservation Areas visible around the site (e.g. Boundary Estate at the lower left of the image)

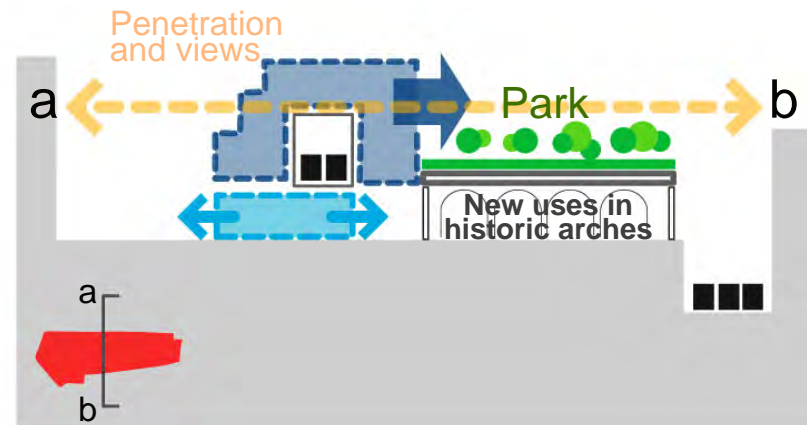


Fig. 45: Indicative section showing penetration and views along the northern edge

### Sustainable design

2.86 Bishopsgate Goods Yard, as a central location, will be linked to excellent public transport access, will reuse previously developed ('brownfield') land and bring important historic structures back into use. In this context, the future development of the site has the potential to deliver many of the key principles of sustainable development as set out in national planning guidance, e.g. in Planning Policy Statement 1: Delivering Sustainable Development, Planning Policy Statement 3: Housing and Planning Policy Guidance 13: Transport.

2.87 The detailed design of future redevelopment proposals should take advantage of the sustainable development context and incorporate high standards of sustainable design and construction as set out in current planning policy.

2.88 To ensure best practice in sustainable design is embedded, the future redevelopment of Bishopsgate Goods Yard should:

- provide a low carbon development which maximises carbon reduction and applies the Mayor's energy hierarchy, with an aim to provide 20% of energy requirements from renewable sources (Mayor's Energy Strategy);
  - include an area-wide energy strategy including localised energy generation;
  - all new homes must meet lifetime homes standards, with 10% able to be adapted to achieve wheelchair accessibility standards;
  - seek to deliver new homes which achieve the Code for Sustainable Homes level 4
- development should achieve BREEAM Excellent standards for retail and office uses;
  - provide a mix of social infrastructure and open space necessary to support a sustainable community;
  - seek to minimise car parking requirements and incorporate a design which prioritises walking, cycling and use of public transport;
  - help to reduce crime, taking into consideration the standards set out in 'Secured by Design';
  - reduce natural resource consumption, including the use of sustainable construction materials;
  - include a site-wide strategy for waste management, recycling and waste recovery throughout the development, that minimises the need for vehicle based waste collection, including consideration of a community waste management facility and segregated litter bins;
  - include sustainable water cycle management, including the incorporation of SuDS, water conservation measures and rainwater harvesting, where feasible;
  - safeguard protected species including black redstarts and provide habitat to encourage their continued presence on site;
  - provide a net gain in biodiversity through measures such as habitat creation within open spaces, for example the park above the Braithwaite Viaduct and through the incorporation of green and brown roofs;
  - re-use existing listed structures and explore the potential to use reclaimed buildings materials.

